

M25 Junction 28

From: Gwyn Church [REDACTED]
Sent: 16 February 2021 16:13
To: M25 Junction 28
Subject: Deadline 3a - Accompanied Site Inspection - Gardens of Peace - Plot 1/8
Attachments: Appendix 1 - Site Access.pdf; Appendix 2 - Development Plan.pdf; Appendix 3 - Plot 1.8 Boundary.pdf; Appendix 4 - Plot 1.8 Overlay.pdf; Appendix 5 - Cadent Detailed Design.pdf

Importance: High

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Deadline, EO

Dear Sir / Madam,

Accompanied Site Inspection Gardens of Peace – Plot 1/8

Ahead of the deadline of Thursday 18th February, please find below and attached relevant information, for the attention of the Inspector, ahead of the Inspectors Accompanied Site Inspection (ASI).

Our client, the Trustees of Gardens of Peace Muslim Cemetery, owns Plot 1/8. We are instructed to nominate the land within Gardens of Peace ownership for an ASI. We provide the relevant information and highlight the concerns of Gardens of Peace as a reasoning for the site nomination. Plot 1/8 is subject to temporary possession with permanent rights and forms part of the cadent gas main route.

We provide the attached plans to aid the ASI:

- Appendix 1 – Site Access
- Appendix 2 – Development plan
- Appendix 3 – Plot 1/8 Boundary
- Appendix 4 – Plot 1/8 Overlay
- Appendix 5 – Cadent Detailed Design

It is proposed that the meeting place for the ASI attendees is to be at the main site entrance to the cemetery (as shown in Appendix 1). Access is readily available and there is parking on site. As the site is now a construction site, all visitors must sign into the site office on arrival. The attendees will be confirmed nearer the time of the ASI and when more information is known in terms of government COVID regulations on group meetings.

From the site entrance, the attendees will walk the proposed route of the internal road (see Appendix 2), to where it meets the boundary of Plot 1/8 (as outlined in Appendix 3), and then follow the Plot 1/8 boundary. From here, Gardens of Peace, and its advisors, will be in a position to explain the design of the development, the cemetery development programme, works that have been undertaken up to the date of the ASI and the effects of the gas main on the development. The concerns of Gardens of Peace in light of the gas main diversion and its effect on the cemetery daily operations and congregations have been provided within previous submissions, however, for the purpose of nominating Gardens of Peace site for an ASI, we take this opportunity to highlight these again and which will be drawn upon during the ASI.

The DCO application identified Plot 1/8, as an area that will be subject to acquisition of permanent rights, thus, and as referred to below, it is assumed all land will be sterilised from its intended use, being internal road, underground services and 512 burial plots. The attached development plan (Appendix 2) provides the development design and when overlaid with the Plot 1/8 boundary (Appendix 4), the severity of the gas main diversion works (Work No. 29) on Gardens of Peace development becomes evident.

By the time the gas main installation works commence, the cemetery development would have been completed and the cemetery will be operational. In terms of what this means for Plot 1/8, and as identified within Appendix 2, Plot 1/8 will be subject to an internal road, a 'service entrance' directly off Colchester Road (A12), a soil storage area, further utilities, part of an overflow car park and land prepared for 512 burial plots. In addition, during 2020, Gardens of Peace commenced some initial ground works, which included installing underground services within the area falling within Plot 1/8.

Furthermore, as a result of the gas main installation works, Plot 1/8 will be severed from the rest of the cemetery, meaning there will be no access for service vehicles off the A12, no access to the workshop / store, soil storage site or to the over flow car park.

We go into greater detail on each element below and how the gas main installation works will detrimentally affect the future operations of the cemetery and whether it is a temporary or permanent issue;

Service Entrance (temporary issue)

Gardens of Peace allowed for a service entrance within its development to ensure the safety of visitors and staff by minimising overlaps between the operations and movements in each user group. Furthermore, having two entrances ensures a clear separation of all service and maintenance traffic from the sensitive occasion of congregations.

The consequence of the closure of the service entrance is that it will cause significant disruption to daily site operations as there will be a need to ensure alternative means to maintaining this separation, including changes to operating hours of the cemetery, impacting on visiting opening times and staff working hours. Should a solution to this issue not be provided, there is a fear that the knock on effect will be the potential fall in demand for burials at Maylands during the period of the installation works.

Soil Storage Area (temporary issue)

The designated soil storage area, required for storing soil from burial plots (and which will subsequently be removed from Maylands on a regular basis), is to be located within the most easterly shaded red area in Appendix 2. As a result of the loss of internal road and service entrance, the soil storage area will need to be relocated to an alternative and accessible area within Maylands to allow for regular collection. However, as you will note from the Appendix 2, there is no such area available. As is evident within Appendix 4, the only solution to Gardens of Peace is to move the soil storage area to the area shaded grey to the west of the plan. However, with this comes a number of significant issues. These will be the temporary loss of burial plots, effect on biodiversity, compaction to soil prepared for burial plots and aesthetic appearance of the burial ground. Furthermore, access for heavy goods vehicles and plant machinery to and from the temporary soil storage site would impact on the roadways, which are only to be used by visitor traffic.

Overflow Parking (temporary issue)

The overflow carpark, which partly falls within Plot 1/8, will need to be relocated to elsewhere on the site, which brings about the same issues highlighted above in relation to the soil storage area. As at commencement of the gas main installation works, the cemetery will be operational and thus the car park fundamental to daily congregations. Thus the car park will need to be relocated which, in light of the loss of an internal road, will create significant disruption to the traffic management and aesthetics of the site.

Not only does this bring rise to safety concerns, but the ongoing requirement to control traffic and the loss of additional burial plots further demonstrates potential interruption to daily operations and the business.

Loss of Internal Road (temporary issue)

As the internal road falls within Plot 1/8, this will also become severed, thus losing access to the workshop / store. The need for access to the workshop / store is fundamental to the daily operations of Maylands and without it, prevents any form of business operating from Maylands. Whilst there have been discussions between Gardens of Peace and Cadent in relation to the requirement of a temporary road, any temporary proposals will provide yet further disruption for the cemetery development in terms of aesthetics, damage to and temporary loss of land prepared for burials, requirement for traffic management and effect on biodiversity.

Disruption to Utility and Site Infrastructure (temporary and permanent issue)

In light of that fact that the cemetery will be operational by the time the gas main works commence, utilities within Plot 1/8 will need to be relocated (see Appendix 5). These include the existing underground electrical, water and telecoms services to the workshop / store, which were installed in 2020 and are now within the middle of the proposed gas main easement. Other services will include the drainage pipes under the internal road and associated manholes and chambers and underground lighting cables.

All services will have to be lifted and relocated to outside Plot 1/8 prior to the commencement of the gas main installation works to ensure the cemetery can continue to operate. Gardens of Peace is concerned with the effect of

the temporary relocation of these services, which will need to be within land identified for burial plots, and accordingly the potential sterilisation of additional land in both the short and long term.

Furthermore, the original development proposal for Maylands was historically amended with the internal road being moved west to reflect the presence of the existing gas main on the eastern boundary of Maylands. However, it is now assumed that following the gas main installation works, the internal road will have to be moved further to the west again to reflect the presence of the new gas main. It is not clear how the site will be redesigned to accommodate these changes.

Loss of Burial Plots (permanent)

As referred to above, the DCO application seeks to permanently acquire rights over Plot 1/8. Accordingly, and without knowing details on the future land use restrictions, despite requesting this from highways England in 2020, it is assumed the land will be sterilised from its intended use, being 512 burial plots, internal road and car park. Gardens of Peace however have been informed it can place and store materials directly over the gas main.

With this in mind, it is assumed the approved cemetery design cannot be reinstated post completion of the gas main installation works, thus prevent additional operation disruption post completion. Furthermore, this brings rise to significant concerns being the financial viability of the cemetery and the inability to meet the requirements of the Muslim community.

With all the above in mind, there will be significant disruption that will detrimentally affect the daily operations to the cemetery.

We hope we have provided all the necessary information, however, should you require any additional information, then please do let me know. We would be grateful if you could please confirm receipt of this email and Appendices 1 – 5.

Yours faithfully

Gwyn

**Gwyn Church BSc (Hons) MRICS FAAV
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GARDENS OF PEACE
MAYLANDS FIELD
COLCHESTER ROAD
ROMFORD
RM3 0NZ

CONSTRUCTION SITE ACCESS

Site Entrance Gates



GARDENS OF PEACE
MAYLANDS FIELD
COLCHESTER ROAD
ROMFORD
RM3 0NZ

M25
Towards Dartford



A12
Towards
Chelmsford

A12
Towards
Romford

M25
Towards M11, M1
Stanstead, Watford

Site Entrance Gates

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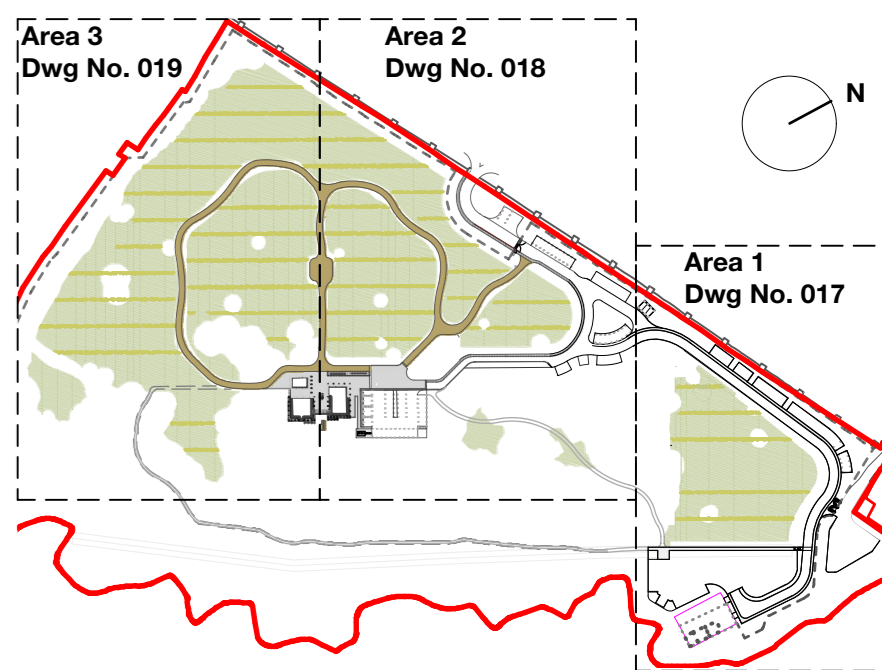
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GROUND PAVEMENT AREAS

Natural stone (Feature Paving)	825 sqm
Resinbound gravel (Path)	1900 sqm
Resin bound gravel (Parking)	1795 sqm
Tegula paving (Harvest colour)	640 sqm
Tarmac (roads)	3580 sqm
Existing Grass/Woodland track	
Grasscrete	1925 sqm
Burial area	
Grass Meadow	
Ornamental hedge	
Buildings	1031.2 sqm
Level Kerb	TBC
Raised Kerb	TBC
Precast Concrete Edgings	81.5 m
Retaining Wall	9.2 m
Graves	8225 No.

NOTE:
REFER TO DWG No. 024, 025, 026 FOR DETAILED SETTING OUT OF KERBS

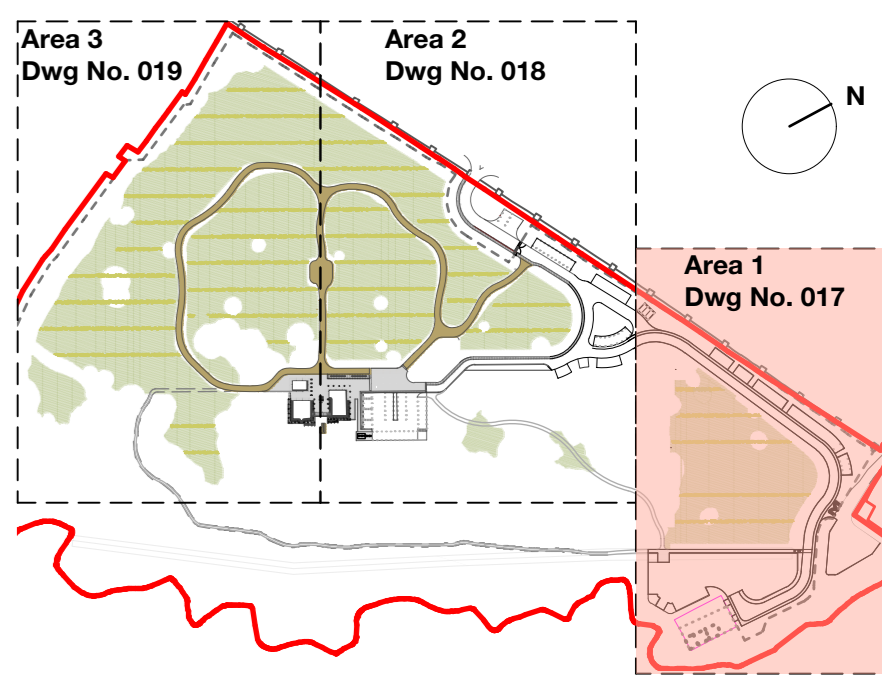


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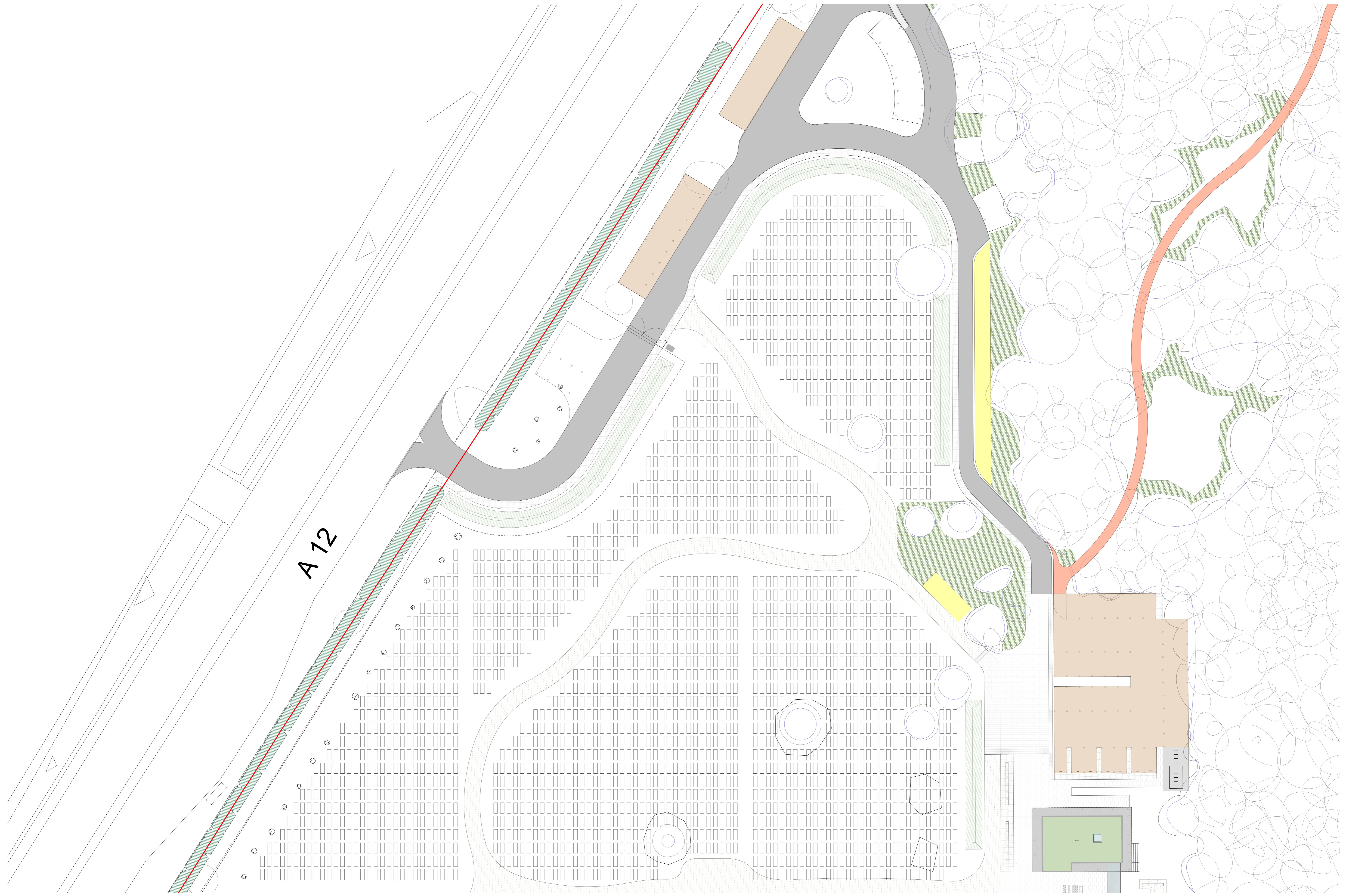
- GROUND PAVEMENT AREAS**
- Resin bound gravel (Parking)
 - Tegula paving (Harvest colour)
 - Tarmac (roads)
 - Existing Grass/Woodland track
 - Proposed native planting
 - Woodland groundflora
 - Grasscrete
 - Burial area
 - Grass Meadow
 - Ornamental hedge
 - Buildings
 - Level Kerb 390 m
 - Raised Kerb 960 m
 - Precast Concrete Edgings
 - Retaining Wall
 - Graves ----- No.

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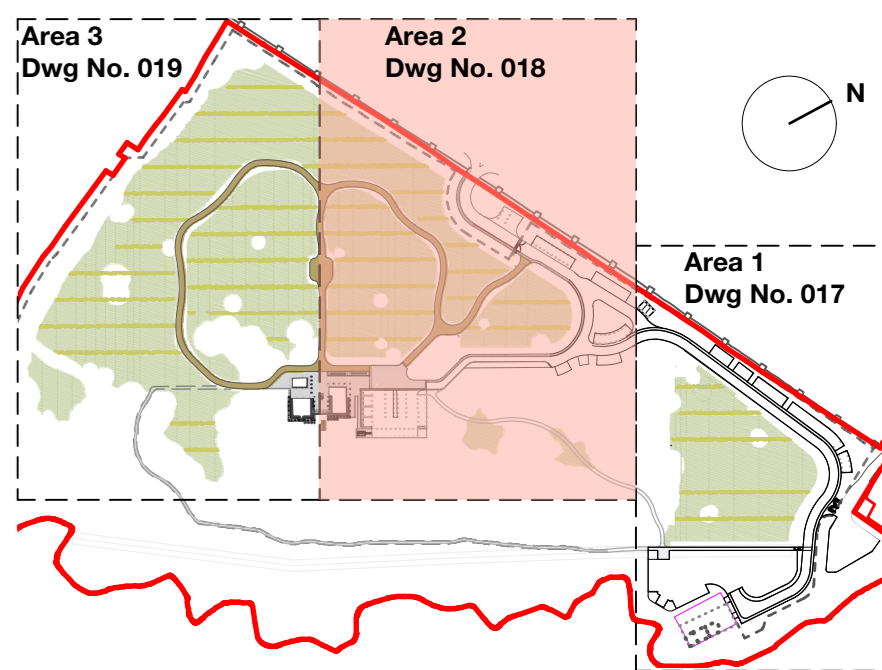


GROUND PAVEMENT AREAS

- Natural stone (Feature Paving)
- Resinbound gravel (Path)
- Resin bound gravel (Parking)
- Tegula paving (Harvest colour)
- Tarmac (roads)
- Existing Grass/Woodland track
- Grasscrete
- Burial area
- Grass Meadow
- Ornamental hedge
- Buildings
- Level Kerb
- Raised Kerb
- Precast Concrete Edgings
- Retaining Wall

Graves ----- No.

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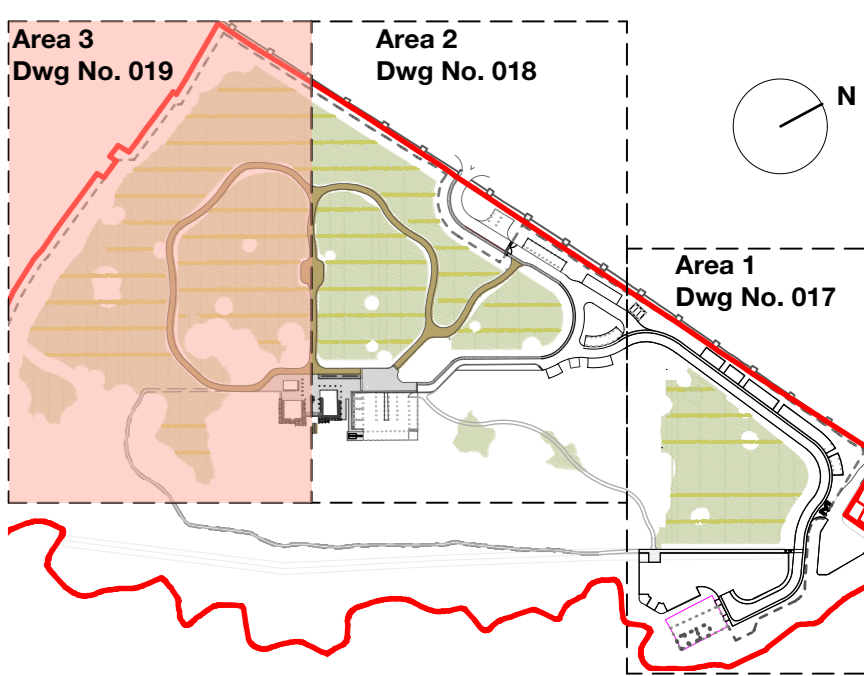


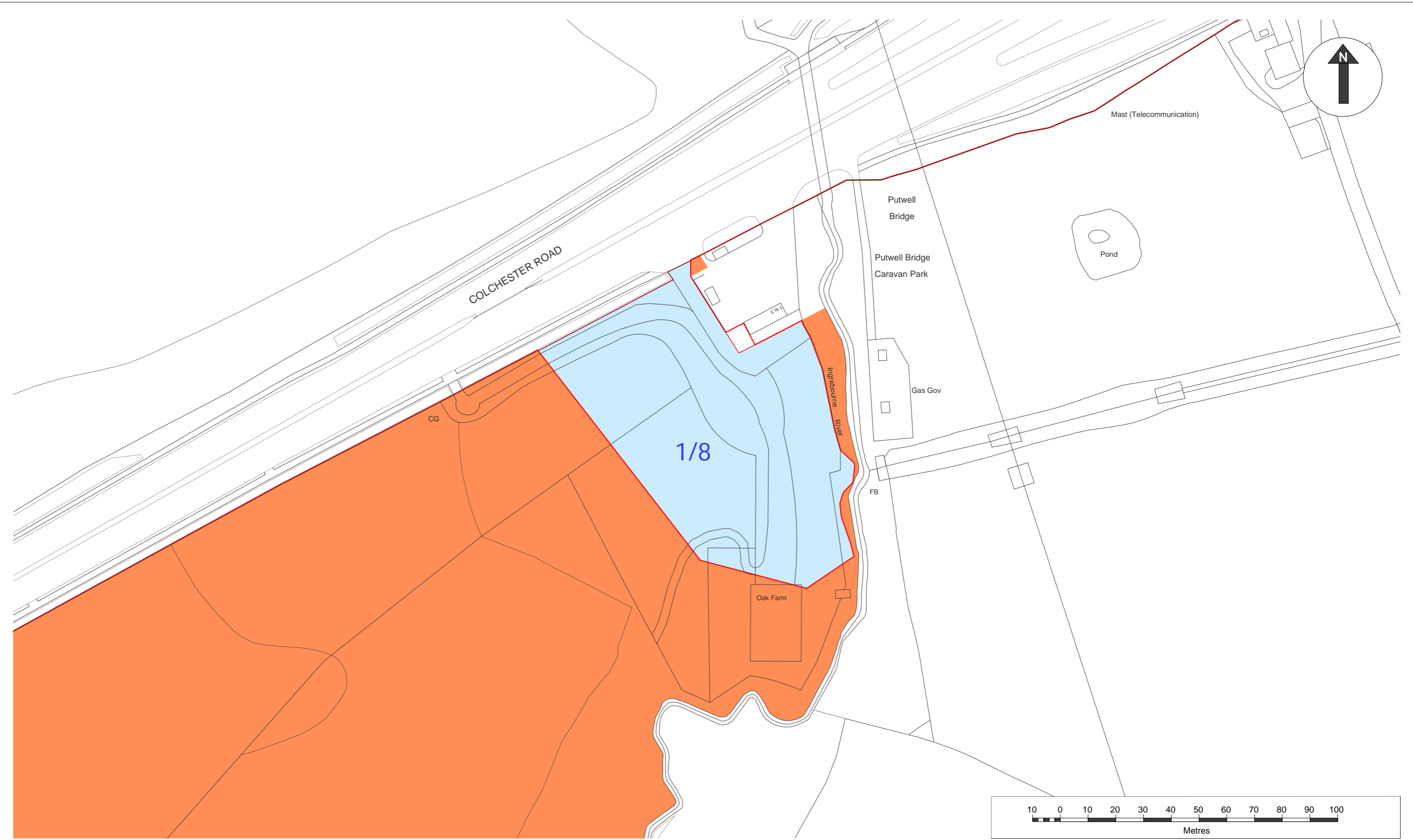
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Graves ----- No.

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

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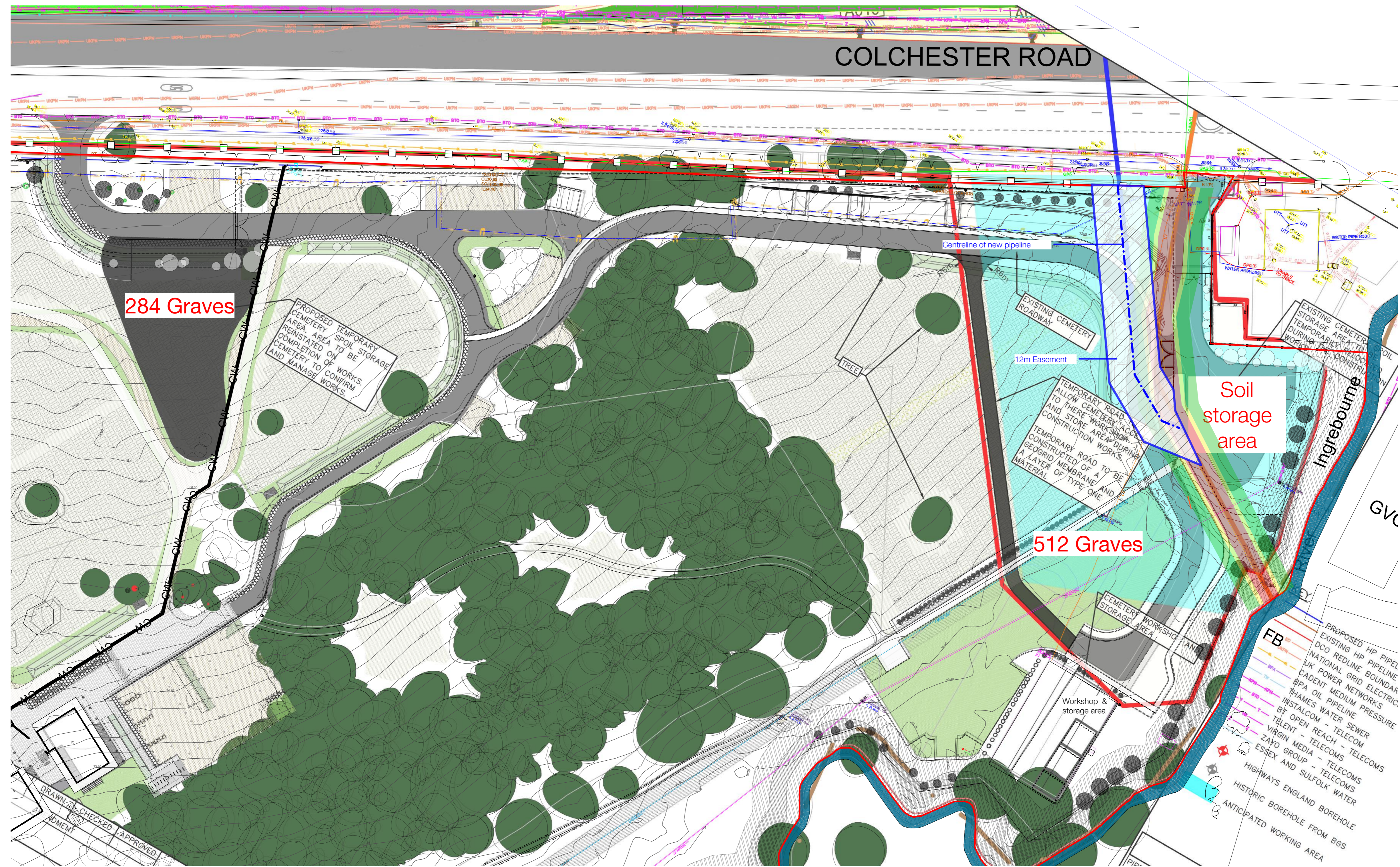


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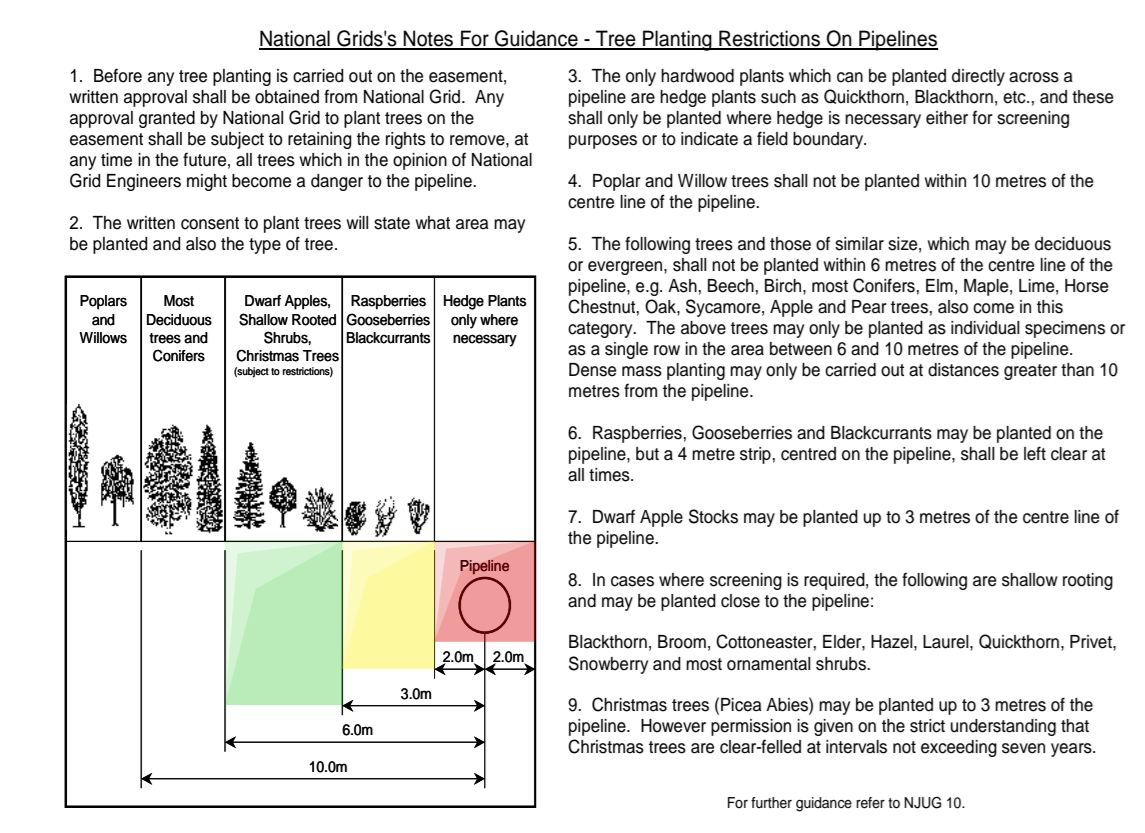
	Order Limits		Other Land not within the Order Limits
	Permanent Title Acquisition		
	Temporary Possession		
	Temporary Possession with Permanent Rights		

Permanent Title Acquisition:	0 sq m	Owner:	Gardens of Peace Muslim Cemetery
Temporary Possession with Permanent Rights:	7,489 sq m	Notes: 1. This plan contains land interests comprised of land within the following title: EGL452021; 2. The plots shown on this plan are owned and occupied by Gardens of Peace Muslim Cemetery; 3. All areas recorded in square metres are rounded to the nearest whole number; 4. The recorded area of Other Land not within the Order Limits is based on the combined areas of the titles listed in Note 1, the full extents of which are not shown on this plan;	
Temporary Possession:	0 sq m		
Other Land not within the Order Limits:	89,177 sq m		

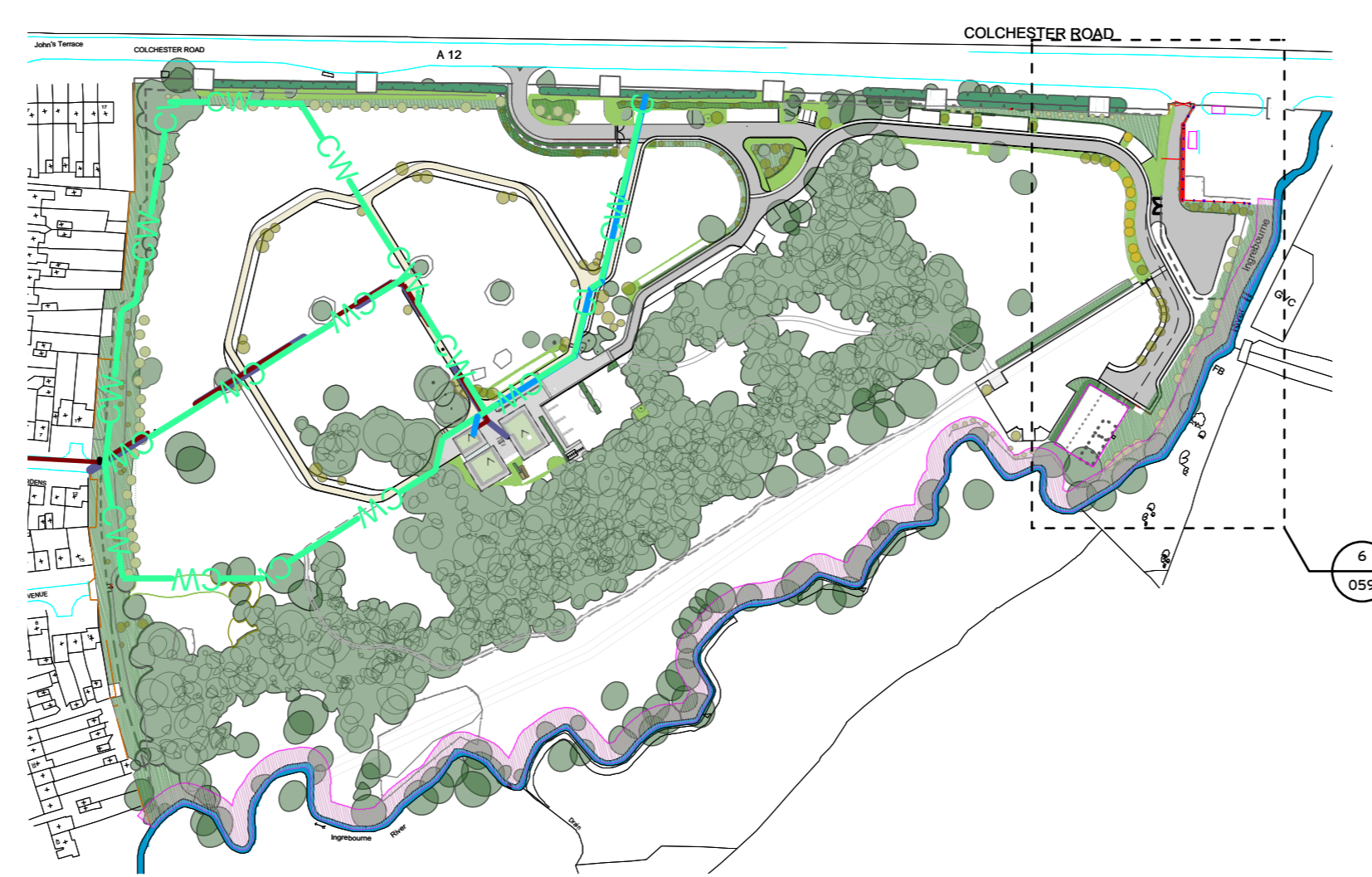
Title: Freehold - Gardens of Peace Muslim Cemetery		
Scale: 1:1250@A3	Date: 14/08/2020	
Drawing No: M25_J28_LNP_2417	Revision: v3.0	
Approved By: MHE	Checked By: ASA	Drawn By: HAB



1 M25/J28 Cadent Detailed Design 1:500



2 Restrictions on pipelines



3 Location Plan 1:2500


- NOTES**
1. PIPELINE DIVERSION DESIGNED IN ACCORDANCE WITH IGEN/TD/1 EDITION 5 & GD/SP/P/10.
 2. WELD TRANSITIONS TO BE IN ACCORDANCE WITH GIS/P16:2020.
 3. ACCESS ROUTES AND WORKING AREAS TO BE CONFIRMED.
 4. PIPELINE DIVERSION WALL THICKNESS WILL BE 15.9mm UNLESS STATED OTHERWISE
 5. REDUNDANT PIPELINE SECTION SHALL BE GROUT FILLED OR REMOVED, TO BE AGREED WITH HIGHWAYS ENGLAND.
 6. PROPOSED ROAD SCHEME IS ASSUMED TO BE HIGH DENSITY TRAFFIC ROUTE (HSTR)
 7. TEMPORARY WORKS TO BE DESIGNED IN ACCORDANCE WITH BS5975 & HSE REQUIREMENTS.
 8. HIGHWAYS TO CONFIRM ALL WORKS THAT WILL BE UNDERTAKEN WITHIN THE REDLINE BOUNDARY.
 9. PROPOSED PIPELINE DIVERSION TO BE FULLY INSTALLED IN HEAVY WALL (PROXIMITY) PIPE.

- KEY**
- PROPOSED HP PIPELINE
 - TEMPORARY HP PIPEWORK
 - UK POWER NETWORKS
 - CADENT MEDIUM PRESSURE PIPELINE
 - BPA OIL PIPELINE
 - THAMES WATER SEWER
 - INSTALCOM - TELECOM
 - BT OPEN REACH - TELECOMS
 - TELENT - TELECOMS
 - VIRGIN MEDIA - TELECOMS
 - ZAYO GROUP - TELECOMS
 - ESSEX AND SULFOLK WATER

DESIGN PARAMETERS

PIPELINE DESIGN PRESSURE (DP)	33.1BARG
MAXIMUM OPERATING PRESSURE (MOP)	33.1 BARG
SAFE OPERATING LIMIT (SOL)	36.41 BARG
MINIMUM OPERATING PRESSURE	0 BARG
PIPELINE NOMINAL DIAMETER (NB)	450 NB
EXISTING PIPELINE WALL THICKNESS	10.31mm
EXISTING PIPELINE MATERIAL GRADE	X42 / L320
MAXIMUM FLOW RATE	TBC KSCH ⁻¹
DESIGN TEMPERATURE (MATERIAL)	-20 TO 60°C
NORMAL OPERATING TEMPERATURE	5 TO 15°C
PIPELINE DESIGN CODE	IGEM/TD/1 ED 5
BUILDING PROXIMITY DISTANCE (R TYPE)	25.6m
BUILDING PROXIMITY DISTANCE (S TYPE)	3M
TEST PRESSURE	TBC
WALL THICKNESS / GRADE (R TYPE)	15.9mm/L415ME
WALL THICKNESS / GRADE (S TYPE)	15.9mm/L415ME

CONSULTANTS



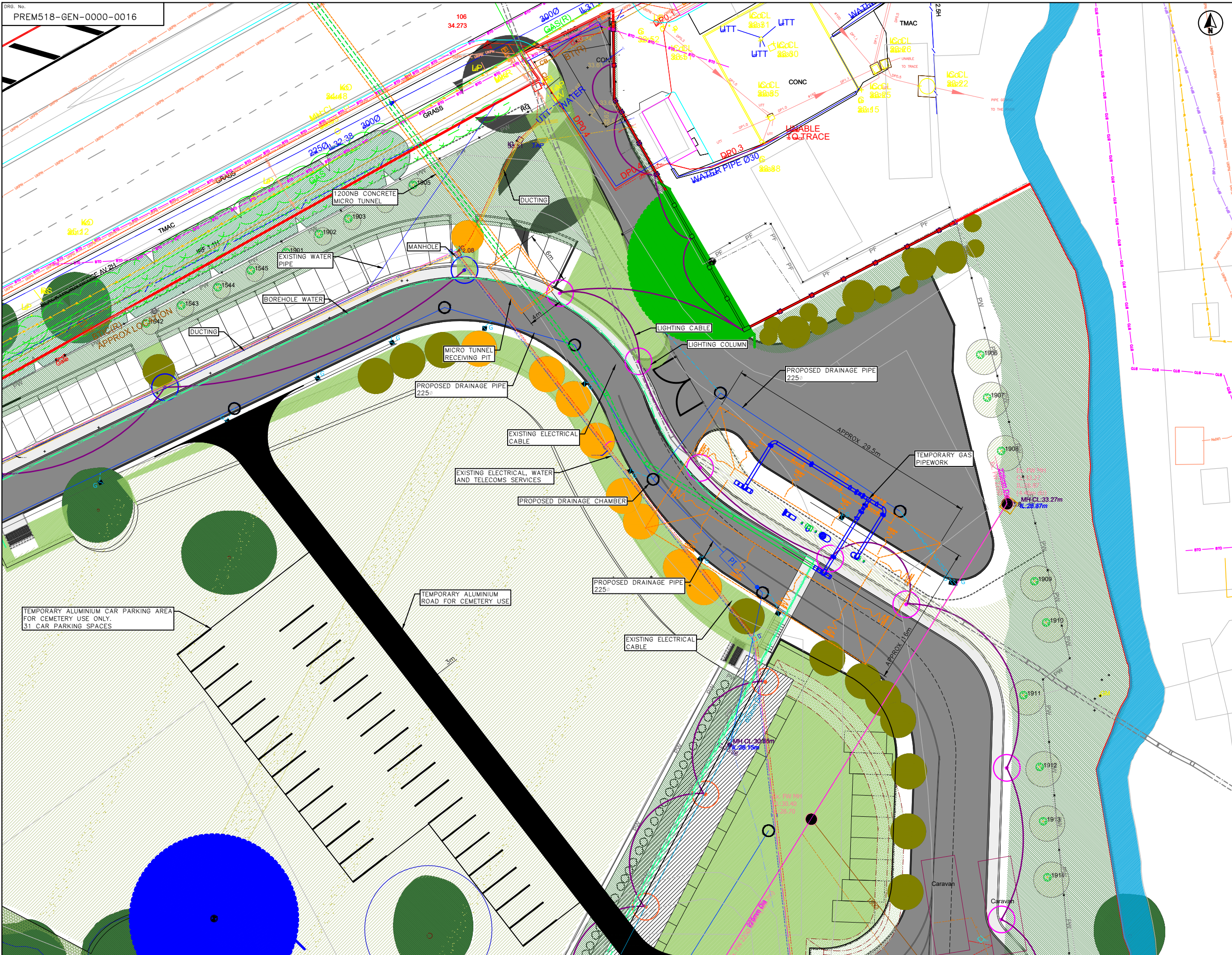
ORIGINATOR	DMP	Premtech Limited
DATE	25/11/20	Cedar House Ivanhoe Business Park Ashby de la Zouch Leicestershire LE65 2UZ
Tel:	01530 563000	
Email:	info@premtchtd.com	
www:	premtchtd.com	

CLIENT



PROJECT
**M25 JUNCTION 28
DETAILED DESIGN**

TITLE
**PROPOSED WORKS INSIDE
CEMETERY**



F	DATE	DRAWN	CHECKED	APPROVED	E	DATE	DRAWN	CHECKED	APPROVED	D	DATE	DRAWN	CHECKED	APPROVED	C	DATE	DRAWN	CHECKED	APPROVED	B	DATE	DRAWN	CHECKED	APPROVED	A	DATE	DRAWN	CHECKED	APPROVED					
	AMENDMENT					AMENDMENT					AMENDMENT					AMENDMENT					AMENDMENT					AMENDMENT					AMENDMENT			

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